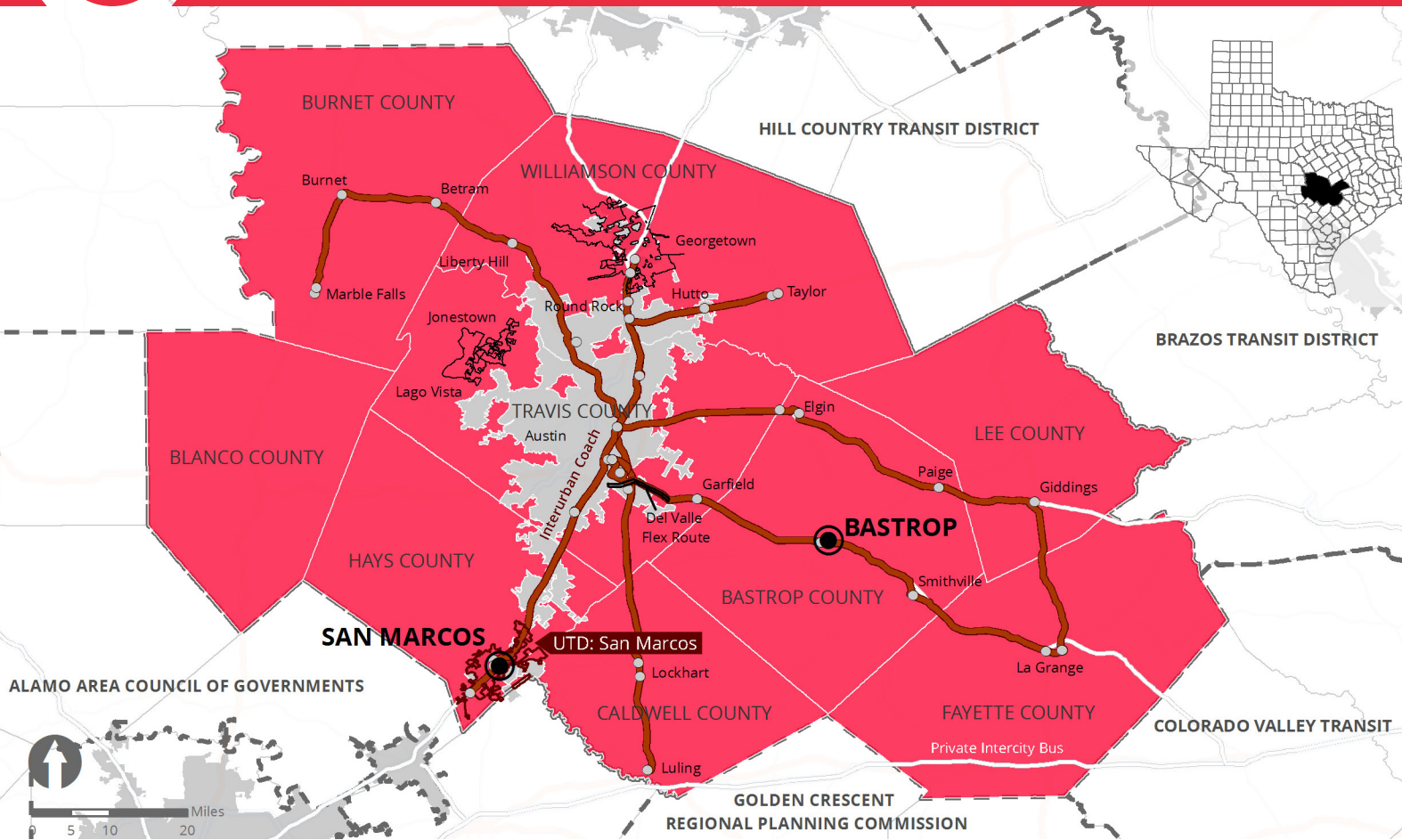


# CAPITAL AREA RURAL TRANSPORTATION SYSTEM



The Capital Area Rural Transportation System (CARTS) was originally organized in 1978 under the Interlocal Cooperation Act of 1971, and was subsequently authorized by Texas Transportation Code Chapter 458 as a Rural Transit District (RTD). It is eligible for formula apportionment of state and federal funds for transit services for the non-urbanized areas of nine counties in central Texas, including Bastrop, Blanco, Burnet, Caldwell, Fayette, Hays, Lee, Travis, and Williamson Counties.

The CARTS family of services includes general public demand-response paratransit (Country Bus) throughout the district and a scheduled regional intercity bus service (Interurban Coach) that connects Austin and 17 surrounding cities to Metro service centers and Amtrak and Greyhound facilities and serves nine CARTS stations in the region. All CARTS stations are Greyhound stations and CARTS operates its Eastside Bus Plaza as the sole Austin terminal for Greyhound and FlixBus. Interurban Express routes serve commuters on routes between Bastrop and Austin and between Austin and San Marcos. The newest member of the family is CARTS Now, an on-demand microtransit service that provides rides within 15 minutes of the passenger request through an app or via phone requests. It now operates in four cities (Bastrop, Lockhart, Taylor and Marble Falls) and will be expanded to other cities in the future.

CARTS collaborates in its region with CapMetro through a Regional Mobility Agreement and with the City of San Marcos through an Interlocal Agreement (to operate The Bus, its local fixed route) under which it provides services of regional importance that are integrated into a regionally connected system. CARTS and CapMetro also jointly operate an Office of Mobility Management to assist in planning, integrating services and filling gaps in service.

CARTS is governed by a board made up of a member from each of the nine county Commissioners Courts in its District.



## Rural Transit District

- Service Area Population<sup>1</sup>: 512,057
- Service Area Land Area<sup>2</sup>: 6,860 Sq. Miles
- Unlinked Passenger Trips<sup>3</sup>: 220,046
- Revenue Fleet<sup>3</sup>: 94 Vehicles
- Transit Staff Counts<sup>5</sup>
  - Non-Operator: 53
  - Operator: 53



## Rural Transit District Services

### Service Information



#### General-Public Demand-Response Service

Known as *Country Bus*.

Available in non-UZAs of Bastrop, Blanco, Burnet, Caldwell, Fayette, Hays, Lee, Travis, and Williamson Counties.

Reservations must be made at least 1 day before the trip (no later than 4 p.m.) and up to 14 days in advance.

##### One-Way Fare Structure:

- Base fare: \$2.00<sup>a</sup>
- Extra fare (in addition to base fare): \$2.00<sup>b</sup>, \$4.00<sup>c</sup>
- Reduced fare: 50% fare<sup>d</sup>

##### Fare Media:



CARTS *NOW* ride-on-demand curb-to-curb service is available between any two points within the cities of Bastrop, Lockhart, Taylor and Marble Falls.

##### One-Way Fare Structure:

- Base fare: \$2.00
- Reduced fare: 50% fare<sup>d</sup>

##### Fare Media:



##### Notes:

- <sup>a</sup> Within the city limit (in town).
- <sup>b</sup> Beyond the city limit (out of town).
- <sup>c</sup> To other counties/areas within service area.
- <sup>d</sup> Children (under 12 years old) accompanied by an adult, ADA eligible individuals, or seniors (65+ years old).



#### Regional Bus Service (Interurban Coach)

Nine bus lines connect Austin and surrounding cities: Bastrop, Bertram, Burnet, Elgin, Georgetown, Giddings, Hutto, La Grange, Liberty Hill, Lockhart, Luling, Marble Falls, Paige, Round Rock, San Marcos, Smithville, and Taylor. The Eastside Bus Plaza operates from 6 a.m.-10 p.m, 365 days a year.

##### One-Way Fare Structure:

- Zone 1: \$2.00 (Intra-county)
- Zone 2: \$4.00 (Inter-county)
- Regular pass: \$6.00 (Day pass), \$88.00 (Monthly pass)
- Reduced fare: 50% fare<sup>d</sup>

##### Fare Media:



#### Commuter Bus Service (Metro Connector)

The Manor/Elgin Express 990 route provides commuter bus service from Downtown Elgin to Downtown Austin. The 1515 Purple Route Express provides commuter service from Bastrop to Downtown Austin.

##### One-Way Fare Structure:

- Base fare (990 Route):  
Elgin to/from Manor: \$2.00  
Capital Metro to/from Manor: \$3.50  
Capital Metro to/from Elgin: \$3.50
- Base fare (1515 Route): See Regional Bus Service fare structure
- Reduced fare (both routes): 50% fare<sup>d</sup>

##### Fare Media:





# Rural Transit District Services

## Service Information



### ADA Paratransit Service

Available within the city limits of Georgetown. Reservations can be made up to 14 days in advance and until 5 p.m. the day before travel.

#### One-Way Fare Structure:

- Base fare: \$2.00
- Reduced fare: 50% fare<sup>a</sup>

#### Fare Media:



#### Notes:

<sup>a</sup> Children (under 12 years old) accompanied by an adult, ADA eligible individuals, or seniors (65+ years old).

## Operating Time (Excluding Holidays)

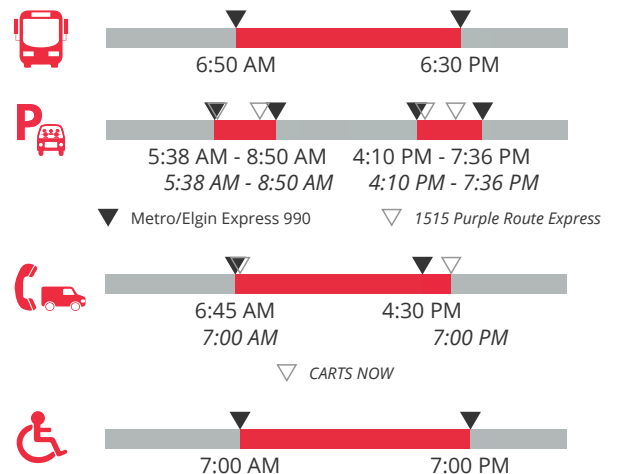


### Operating Days by Mode

	MON	TUE	WED	THU	FRI		
	MON	TUE	WED	THU	FRI		
	MON	TUE	WED	THU	FRI		
	MON	TUE	WED	THU	FRI		



### Operating Hours by Mode



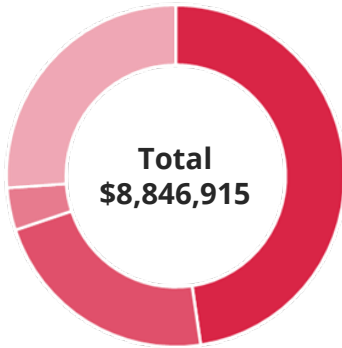


Address | 5300 Tucker Hill Ln  
Cedar Creek, TX  
78612

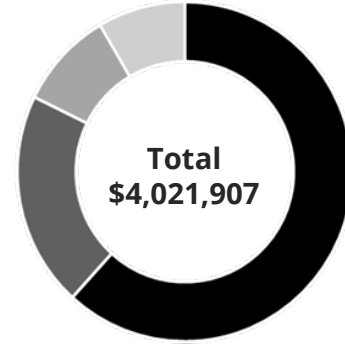
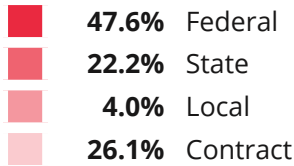
Contact | David Marsh  
General Manager  
Phone: (512) 481-1011  
E-mail: dave@ridecarts.com

## FY 2023 Financial Status<sup>3</sup>

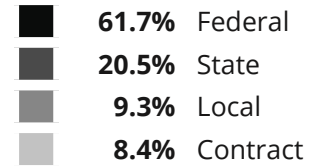
### \$+ Revenues in Fiscal Year 2023



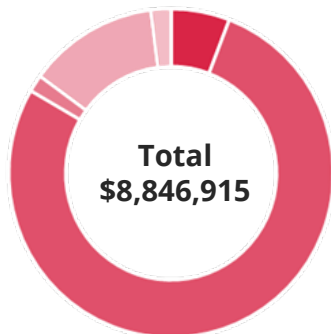
#### CARTS



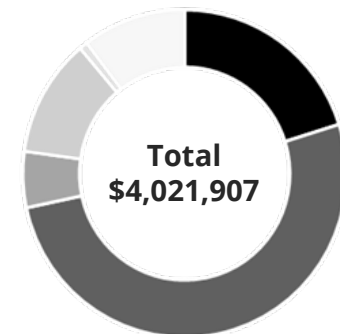
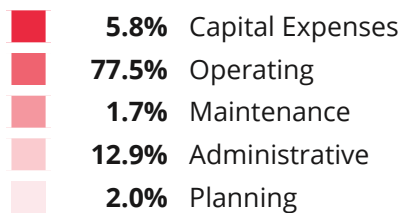
#### Rural Average



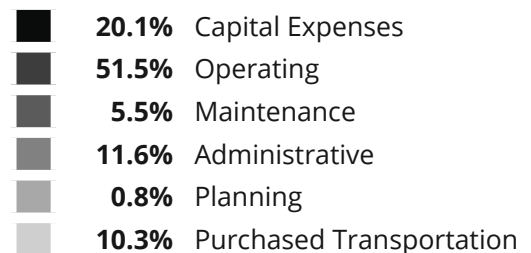
### \$- Expenditures in Fiscal Year 2023



#### CARTS



#### Rural Average





**Address** | 5300 Tucker Hill Ln  
Cedar Creek, TX  
78612

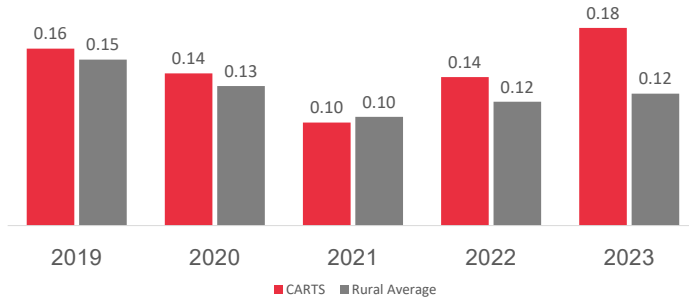
**Contact** | David Marsh  
General Manager  
Phone: (512) 481-1011  
E-mail: dave@ridecarts.com

## FY 2019–2023 Performance Measures<sup>3</sup>

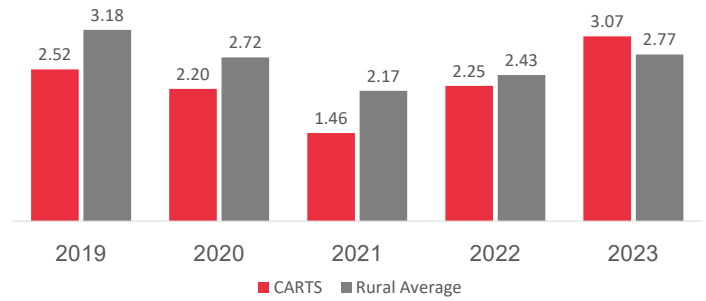


### Productivity in the Past 5 Years

#### Unlinked Passenger Trips per Revenue Mile

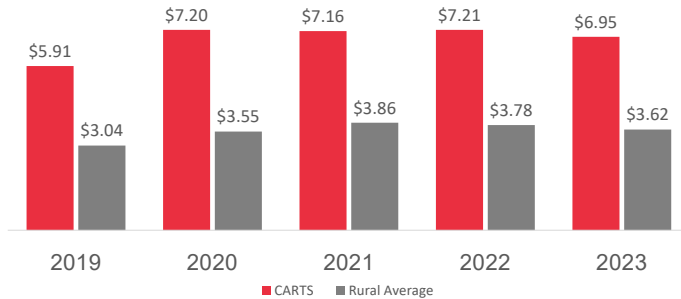


#### Unlinked Passenger Trips per Revenue Hour

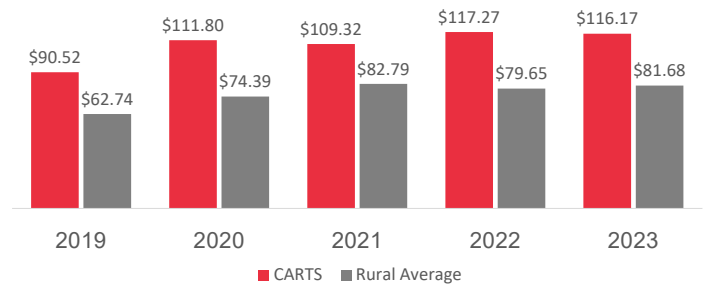


### Efficiency in the Past 5 Years

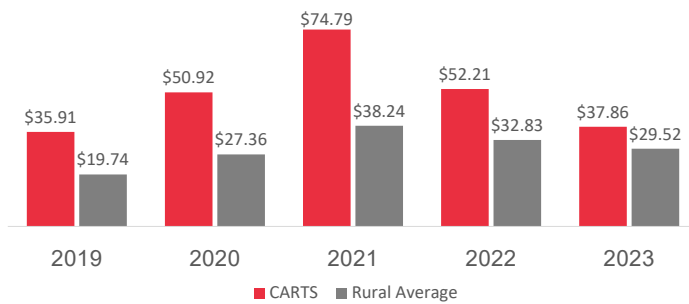
#### Operating Cost per Revenue Mile



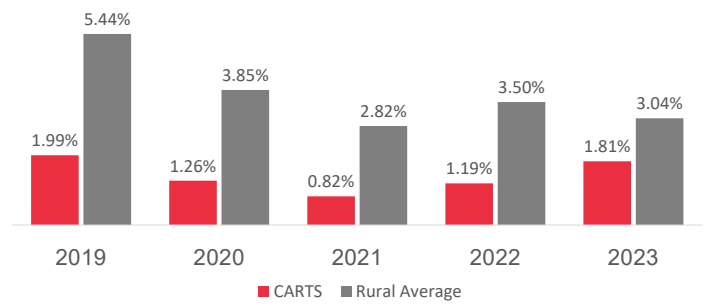
#### Operating Cost per Revenue Hour



#### Operating Cost per Passenger



#### Fare Recovery Ratio



## Projects in 2024-2028<sup>4</sup>

Implementation Year	Project Name	Cost
2023-2024	Microtransit Country Bus Pilot	\$383,000
2023-2024	Website Construction	\$85,000
2023-2024	Smithville Station Rehab	\$1,299,000
2023	Flatonia Station Planning	\$88,000
2023	Complete Security/IT Project	\$281,835
2023	Purchase Buses	\$296,212
2023	EBP Phase 3 - Intercity Traffic Improvements	\$677,000
2023	San Marcos Transit Operations	\$2,381,335
2023	Operating Capital Metro Services	\$2,000,000
2024-2025	Expand Microtransit to Candidate Cities	Not Available

### Data Sources:

<sup>1</sup> TTI estimation based on (1) the 2018-2022 American Community Survey 5-Year Estimates, (2) 2020 Census Summary File 1, and (3) the U.S. Census Bureau 2020 TIGER/Line Shapefiles.

<sup>2</sup> TTI estimation based on the U.S. Census Bureau 2020 TIGER/Line Shapefiles.

<sup>3</sup> Texas Department of Transportation, Public Transportation Division, PTN-128 FY2019-2023 Data.

<sup>4</sup> TTI survey data.

<sup>5</sup> TTI survey data. Staff counts include the number of full-time employees and part-time employees as of the last day in the fiscal year. The agency reported both public agency and private contractor employees who are dedicated at least 50% to the management or operation of the public transit services described in the profile. Staff counts does not include volunteers or employees at another transit district from which service is purchased.

### Last Updated:

August 2024